

APPENDIX “B”

A HISTORY OF DOWNTOWN REDEVELOPMENT PLANS

A History of Downtown Redevelopment Plans

1965 DOWNTOWN PLAN: The first Downtown Plan was developed in 1965 by the firm Harland Bartholomew and Associates. (See Figure A) The consultant recommended that the Robbinsdale Downtown plan should include programs for improvements of buildings and considerations for improved traffic and parking as well as pedestrian circulation between the commercial establishments and parking that was safe, convenient, and pleasant. The development of streets and parking areas were proposed to be designed to avoid conflict and would create interesting vistas that would improve the general appearance of the area.

PROPOSED PLAN SUMMARY

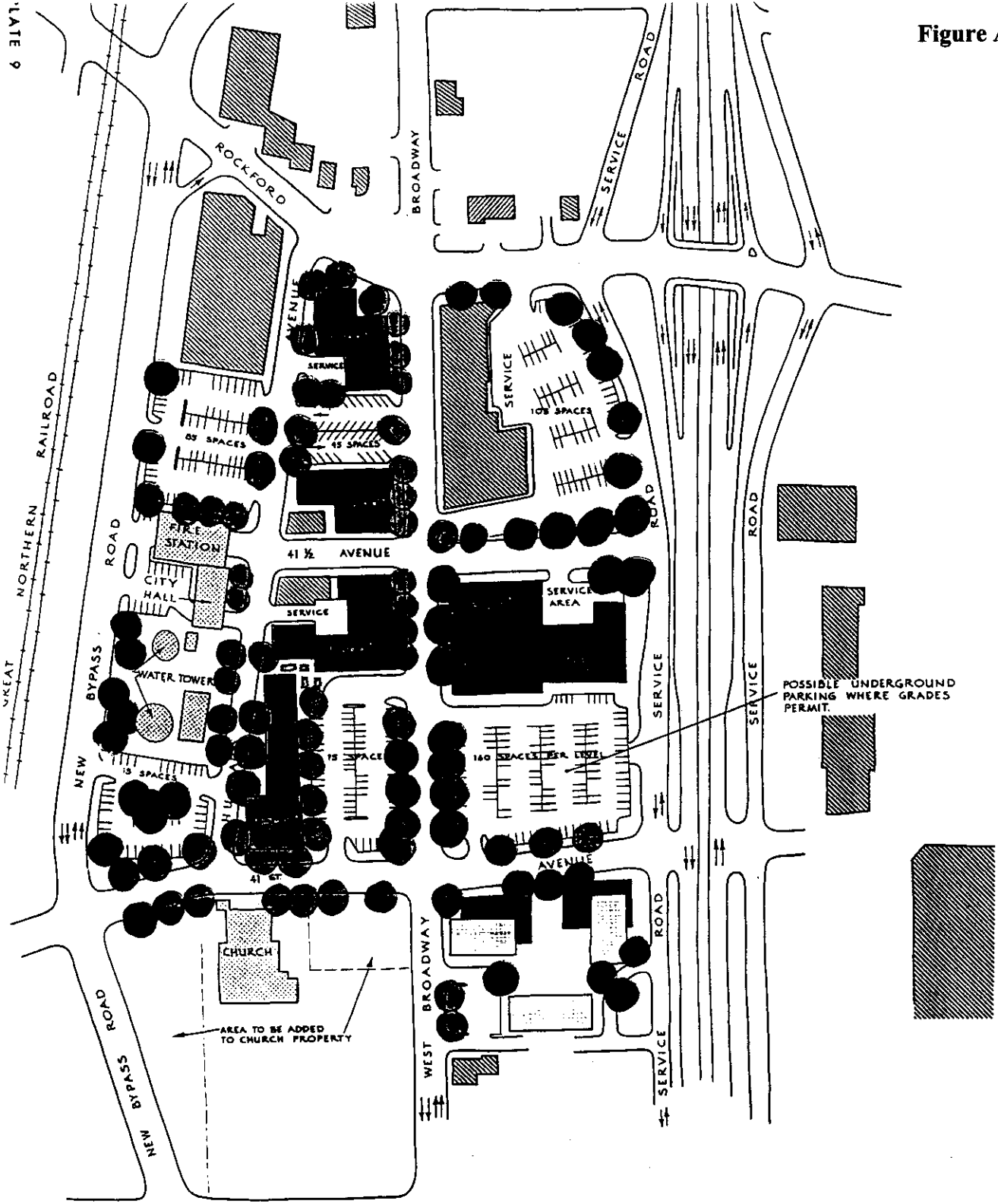
The plan for the central business area was based upon the redevelopment and intensification of primary commercial uses along West Broadway from 41st Avenue to Rockford Road. Off-street parking facilities were grouped in rather large parking lots that were both convenient to the commercial buildings and evenly distributed throughout the business area. An extension of the Police and Fire Station was shown to provide a separate wing for City Hall administrative offices. This tied in with the water tower and treatment plant, which would provide a buffer of public use along one side of the business district.

South of 41st Avenue, an apartment building complex was indicated with possibly commercial uses occupying the lower levels. This was identified as an area, which could also be developed into office buildings. The block occupied by the Sacred Heart Church was identified as eventually being used entirely for that specific purpose.

No significant change was shown on the plans for the fringe area north of Rockford Road. The plan gave support to commercial facilities to continue to exist and develop in this area. However, the Downtown core area of the City was supported as the principal center for commercial activity by the plan.

1970 SUGGESTED DOWNTOWN PLAN: A plan authorized by business community with limited support from City Officials and staff was prepared. The major thrust of this proposal was limited to improvements and beautification within the public right-of-way. Because of the lack of a ready funding source, the plan was never implemented.

Figure A



PRELIMINARY PLAN
CENTRAL BUSINESS DISTRICT
 ROBBINSDALE, MINNESOTA

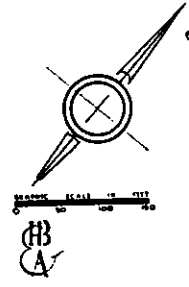
URBAN PLANNING GRANT PROJECT NO. P-11
 THE REPAIRS OF THE MAP WAS FURNISHED BY
 THE URBAN PLANNING GRANT PROJECT FROM
 THE FEDERAL AND STATE FUNDS. PROJECT UNDER
 THE PLANNING OF SECTION 101 OF THE HOUSING
 AND URBAN DEVELOPMENT ACT OF 1954, AS AMENDED, AUTHORITY OF
 THE BOARD OF COMMUNITY PLANNING, MINNESOTA
 DEPARTMENT OF HUMAN DEVELOPMENT

CITY PLANNING COMMISSION
 ROBBINSDALE, MINNESOTA

HARLAND BARTHOLOMEW AND ASSOCIATES
 CITY PLANNERS, CIVIL ENGINEERS, LANDSCAPE ARCHITECTS
 SAINT LOUIS, APRIL, 1965 MISSOURI

LEGEND

- PROPOSED COMMERCIAL
- EXISTING COMMERCIAL
- PROPOSED APARTMENTS
- PROPOSED PARKING
(840 SPACES AT GRADE - 150 UNDERGROUND)
- PUBLIC & SEMI-PUBLIC



1977 DOWNTOWN PLAN: The third plan was developed by Midwest Research in the year of 1977. (See **Figure B**) The plan called “Project 4” identified the downtown as District One – West Broadway Business District. The concept plan of this specific area was thought to become a focal point for the City, which provides an identifiable commercial center with compatible, complementary uses, and supportive parking. Building placement and uses were suggested to allow for both a visual and functional interrelationship between individual operations. The goal was to increase the collective “drawing” power of the commercial core of the City. The West Broadway Business Redevelopment District Concept Plan was based on the following principles:

Establishing a “nucleus” for the District surrounding Broadway.

Develop an urban (zero setback) building massing concept in the nucleus area.

Improve the parking conditions in the nucleus with “pooled parking lots eliminating the inefficiencies of lot line limitations on parking and circulation design.

Allow freestanding buildings and on site parking to develop and exist around the nucleus where circulation and an open space concept can complement the development.

Minimize disturbances to enterprises which have made substantial reinvestment in their properties and whose uses are supportive of the commercial core.

LAND USE DESIGN GOAL: Establish a business district which is composed of compatible uses having a city-wide service and are appropriate for an “urban” central business district.

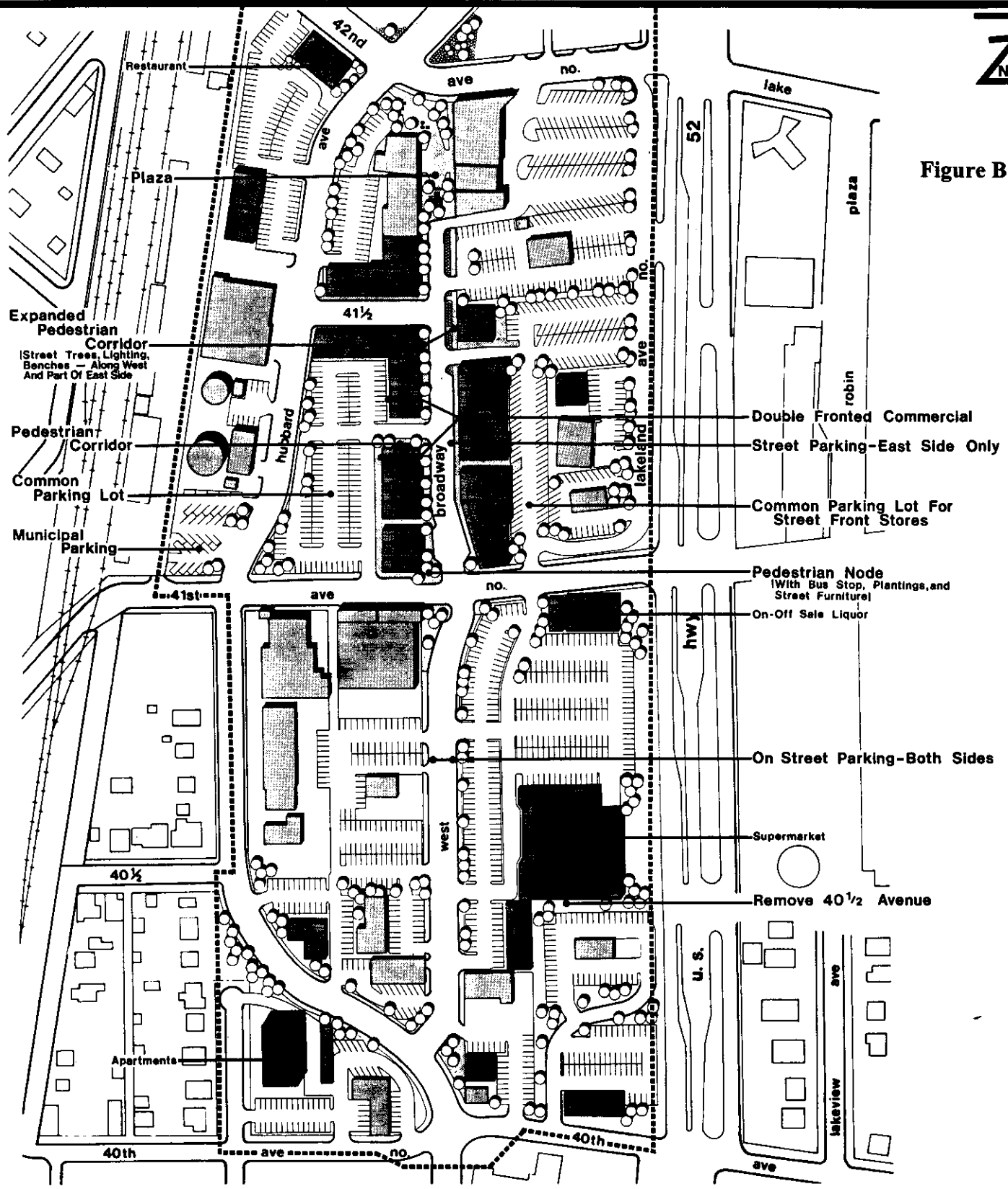
BUILDING GROUPING DESIGN GOAL: General building groupings and specific building placement was thought to visually and functionally reflect a sense of order. Buildings are simultaneously reflecting the degree of structural autonomy and yet maintain area interdependence and identity. Maximum site utilization is a mandatory requirement.

PARKING DESIGN GOAL: In all areas where redevelopment occurs, the quality of parking provided was to be increased. Parking was suggested to be integrated into all new development sites and was thought to aesthetically complement any area projecting a commonly established theme.

SIGNAGE DESIGN GOAL: Provide an overall system of public and private signage that is unique and unifies the West Broadway Business area, but which simultaneously facilitates individual use identification.



Figure B



**West Broadway
Business District
District 1**

----- Planning District Boundary

30 0 60 120 180 240 300
June 1977
This map is for planning purposes only and should not be used when precise measurement is required.

**Redevelopment Project 4
Robbinsdale Minnesota**



LIGHTING DESIGN GOAL: Lighting should be appropriate for the intended activity and should carry a common theme and projection.

STREETSCAPE IMPROVEMENT GOALS: The Project Four Plan for the West Broadway District called for substantial landscaping and boulevard treatment, as well as a small plaza to be provided along West Broadway between 40th and 42nd ½ Avenue. These boulevard and landscape improvements were suggested to be made on a project by project basis.

1984 DOWNTOWN PLAN: The Hubbard Place Plan was a direct result of the 1977 plan which recommended that the roadways be configured to create a ring road in the downtown and closing off West Broadway. **(See Figure C and D)** The City acquired property based on an earlier recommendation from the 1977 plan. The improvements were never completed but the City still owned the property located in the approximate area of 42nd and West Broadway and had the option to create a cohesive central parking lot that would utilize Hubbard Street to its maximum potential.

1990 DOWNTOWN PLAN: In 1990 the City was introduced to the concept of Light Rail Transit (LRT). The 1990 plan for Downtown would include an LRT facility. The City over time would have to protect its interests by planning how the LRT facility would be incorporated into the downtown if it were to become reality someday. **See Resolutions and Maps in Chapter 9 – Transportation.**

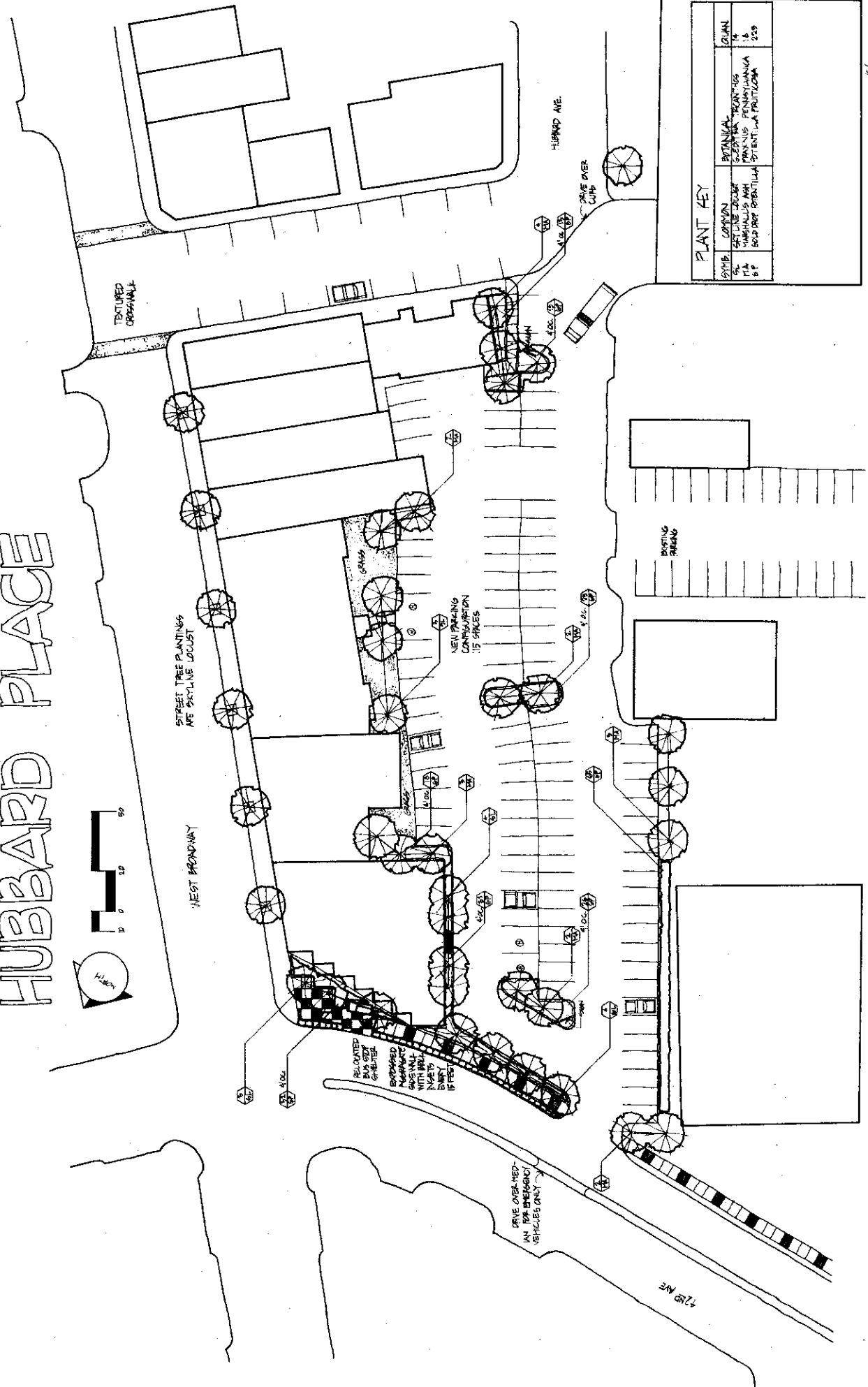
1991 DOWNTOWN PLAN: In 1991 a redevelopment plan and feasibility plan study was completed for the Downtown West Broadway area by Dalgren, Shardlow, and Uban, Inc., RLK Associates, Inc. and McCombs, Frank, Roos, Associates, Inc. **(See Figure E)** The study was a feasibility study for improvements to West Broadway Avenue between 40th and 42nd Avenues and an effort to visualize and articulate a long-range plan for Downtown Robbinsdale. This plan will provide guidance for future redevelopment decisions and a framework to ensure that the City's goals are met overtime. The Economic Development Authority, Downtown Design Committee, and the City Staff guided this effort. A Master Plan was created to insure that the improvements made to West Broadway would be integrated into future redevelopment of the downtown and expansion of a streetscape system.

Objectives and Goals

- A. Create a positive and unique identity for Downtown Robbinsdale by developing a strong theme to be used in all streetscape elements and tie into the architectural character of the downtown.
- B. Improve the pedestrian and vehicular environment to create a safe, functional, attractive City.
- C. Develop a community focus in Downtown Robbinsdale that would include a public gathering place, infrastructure investment, and activity generators.

Figure C

HUBBARD PLACE

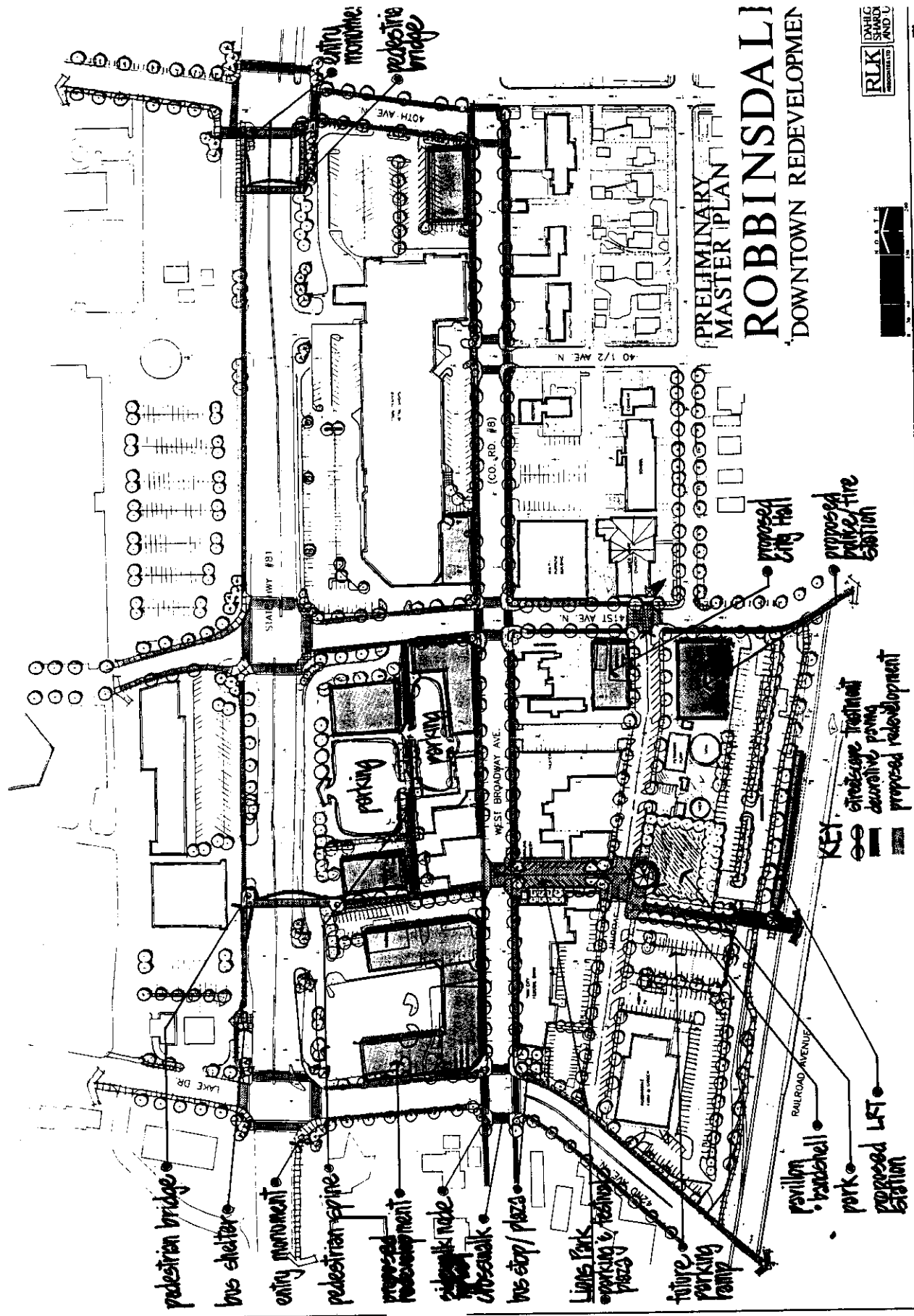


PLANT KEY

| SYMBOL | COMMON NAME | BOTANICAL NAME | QUANTITY |
|----------|----------------|----------------|----------|
| (Symbol) | SPRING DOGWOOD | SPRING DOGWOOD | 14 |
| (Symbol) | HUMMELBUSH | HUMMELBUSH | 16 |
| (Symbol) | BOYD BERRY | BOYD BERRY | 259 |

9/84

Figure E



- D. Strengthen the relationship between West Broadway and County Road 81.
- E. Protect the interests of the City and the property owners in planning for Light Rail Transit (LRT).
- F. Reconstruct West Broadway Avenue to be an attractive, function, pedestrian-oriented “Main Street”.

The intention of the Master Plan was to illustrate ideas and to establish a framework to assist the City with making redevelopment decisions for the downtown. The plans objective was to ensure that the improvements made on West Broadway would be integrated into future redevelopment of the Downtown and expansion of the streetscape system.

1993 Hyett Palma Market Study: The consultant’s identified Robbinsdale’s best asset as its traditional, historic West Broadway. The study suggested rebuilding, infill, an increase in the density of nearby residential housing, add new stores and provide services to continue the “hometown” mix the City already had. The market study determined that the traditional hometown main street would be the City’s niche in the market.

The following is a summary of recommendations from the market study:

Downtown Robbinsdale should be positioned as the community’s historic retail center and everybody’s neighborhood center, with a collection of quality historic commercial buildings, modern shopping centers an enhanced streetscape, and a growing range of businesses. Downtown Robbinsdale should be positioned as the community’s largest retail center serving the needs of local residents, North Medical Memorial Center, and a growing number of annual visitors.

Downtown Robbinsdale should be positioned as Robbinsdale’s office center. Downtown offers an ideal office environment for office occupants that provide personal services, medical-related services, and business services to clients located throughout the community and primary trade area. Robbinsdale should seek the convenience of an office center in proximity to a broad offering of supportive retail and business-to-business goods and services.

Downtown Robbinsdale should be positioned as the community’s most convenient residential center, with a growing range of retail support services, an improving pedestrian environment, convenient employment opportunities, and an attractive office environment. Downtown has the key ingredients of a quality housing environment. Increased attention should be given, in the future, to developing housing for persons working in and near Downtown, and non-seniors who desire the ambience and convenience of living within a commercial district.

1993 ECONOMIC ENHANCEMENT STRATEGY FOR DOWNTOWN: In 1993, as a result of the Hyett-Palma study, the City commissioned an Economic Enhancement Strategy for Downtown Robbinsdale. The main recommendation was that downtown Robbinsdale be positioned as the community's historic retail center and everybody's neighborhood center, with a small town, hometown appeal. As part of the Real Estate Enhancement recommendation from 1991 market study was the suggestion to set design guidelines for the West Broadway area. These guidelines were to provide assistance to owners making changes to their own buildings and to developers proposing new development projects with the goal of obtaining the traditional architectural main street look and feel.

In 1995, the REDA commissioned Tomten Environmental Design to develop design guidelines.

In 1996, the REDA approved the West Broadway design guidelines

In 1997, the West Broadway design guidelines became an ordinance.